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**Environmental Section/Highway Division/Project Delivery Bureau**  
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## **SECTION 106 PROJECT REVIEW MEMORANDUM**

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To: Project File; VT SHPO

Date: 5/25/2023

Subject: NO ADVERSE EFFECT

Project Name: Northfield

Project Number: BF 0241(58)

Location: Bridge No. 60, Northfield Village, Northfield, Washington County, Vermont.

Distribution: Laura Trieschmann, State Historic Preservation Officer  
Lee Goldstein, VTrans Environmental Specialist

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The Vermont Agency of Transportation (VTrans) has reviewed this project according to the standards and procedures detailed in the *Programmatic Agreement Among the Federal Highway Administration, the Vermont State Historic Preservation Officer, the Advisory Council on Historic Preservation, and the Vermont Agency of Transportation Regarding the Federal-Aid Highway Program in Vermont* executed in 2021 (2021 PA). Completion of this form in accordance with the 2021 PA demonstrates that FHWA has satisfied its Section 106 responsibilities for this project.

### Project Description and Location

Bridge No. 60 carries Vermont Route 12 over the Dog River in the downtown area of Northfield in Washington County, Vermont. Originally constructed in 1926, Bridge No. 60 was substantially repaired in 1958. Since then, Bridge No. 60 has fallen into disrepair and is currently rated a 5/10 for deck condition, a 4/10 for substructure and a 4/10 for superstructure condition. Plans dated 11/18/2021 propose a full removal and replacement upon the existing alignment. A road closure at this location will take 12 weeks during construction to avoid an off-alignment

construction that could potentially impact local properties and other resources. The existing 3-Span 111ft concrete T-Beam bridge will be replaced with a single span 140 ft steel girder bridge. The new foundations will be constructed approximately 15 feet behind the existing spread footing abutments. The new bridge will be founded on a H pile deep foundation drilled into bedrock. The existing piers will be removed to below the streambed and abutments removed down to an elevation of approximately 716', while retaining stone fill to armor the channel slope between the existing and proposed.

New streetlights matching the existing design, color, and materials of streetlights located throughout the historic district will also be installed. Utility relocation and burial will also be included as part of this project.

### APE Description

In defining the project Area of Potential Effect (APE) and determining appropriate identification efforts, the potential direct, indirect, and cumulative effects of the project have been considered, including the possible effects to known or potential historic and/or archaeologically sensitive properties and their aspects of integrity both within and beyond the project limits based on the scope, scale, nature, setting, topography, and other environmental factors associated with the project, such as views from and towards the project area and the potential for long-term effects. In the APE map(s) below, the area(s) of ground disturbing activities and/or direct impacts will be outlined in red and the overall APE will be indicated with transparent shaded yellow. Please refer to **Figure 4** for a visual representation of the direct and indirect APE. Plans used for the review of this document are dated 11/18/2021.

### Archaeological Resources

A field visit and archaeological resource assessment (ARA) was conducted by VTrans Senior Archaeologist Brennan Gauthier in 2019 in order to identify archaeologically sensitive areas in a broad area around Bridge No. 60. Heavy disturbance due to industrial activity and bridge construction was noted in three quadrants and one area of sensitivity was identified to the northeast. The area was once home to series of mills that operated in the village in the 19<sup>th</sup> Century and elements related to a retaining wall were identified during the field inspection. However, the area falls well outside of the project footprint and likely have lost integrity due to the demolition of the mills in the 20<sup>th</sup> Century. As currently planned, there are no concerns for impacts to the retaining wall or any other archaeologically sensitive area. Please refer to the attached ARA for more details.

### Above-Ground Historic Resources

Originally constructed in 1926, then subsequently rehabilitated in 1958, Bridge No. 60 is a continuous span reinforced concrete T-beam bridge that carries Vermont Route 12 over the Dog River in Northfield Village. The original ornamental railings of the structure have been completely replaced with steel tube railings common in materials, design, and construction. Losing this significant character-defining feature and other features of the original concrete deck design has compromised the historic integrity of the structure. Consequently, Bridge No. 60 does not meet the registration requirements for inclusion in the National Register of Historic Places (NRHP) under the *Metal Truss, Masonry and Concrete Bridges of Vermont: 1820-1978* Multiple Property Documentation Form (2019).

Directly south of Bridge No. 60 is the Depot Square Historic District, which is listed in the Vermont State Register of Historic Places (*Figure 9*). At the southeastern corner of Bridge No. 60 stands three contributing resources to this historic district - 9 East Street, 28 N. Main Street, and 38 N. Main Street (*Figures 10-11*). Today, these three buildings appear to retain their historic integrity. When originally surveyed in 1980, the Depot Square district was described as “Ravaged by fire, ... anchored by only two 19th century landmarks, the United Church (1836) and the old Vermont Central Railroad station (c.1858). The square is further defined by 20th century commercial infill.” This description remains accurate today, with even more commercial infill present. Due to the limited scope and impacts of this project, a new survey of the Depot Square Historic District was beyond the scale of effort commensurate with the project’s potential to affect historic properties but will be necessary to determine changes to the district boundaries and contributing resources over the past 40+ years, including a conclusive determination on the district’s integrity and NRHP-eligibility. Nonetheless, VTrans has determined that district appears preliminarily eligible for inclusion in the National Register despite intrusions, with the previously mentioned caveat for additional survey and research work.

### Public Participation

Numerous remote public meetings have been held on this project to provide information on its scope, design, anticipated impacts, and to also solicit comments, including:

- Alternatives Presentation Meeting - February 9, 2021
- Project Update & Design Features Meeting - January 25, 2022
- Northfield Select Board Seeking Comments on New Main Street Bridge - February 2022

Feedback from interested parties and public comments on the proposed project have been overwhelmingly positive. The proposed bridge railing design and streetlights were selected in close collaboration with the town. There are no known public concerns with the project at this time.

### Analysis

The proposed project activities will not result in an adverse effect to the preliminarily NRHP-eligible Depot Square Historic District, which include sidewalk and utility relocation at three buildings near the southeastern corner of Bridge No. 60, which are contributing resources to the historic district - 9 East Street, 28 N. Main Street, and 38 N. Main Street. Additionally, a tree will be removed from the building at 38 N. Main Street and an extant retaining wall will be reconstructed to make bridge construction and maintenance feasible.

Under Section 106 regulations—36 CFR Part 800.5(a)(1)—adverse effects occur when an undertaking may directly or indirectly alter characteristics of a historic property that qualify it for inclusion in the National Register. 36 CFR Part 800.5(a)(2) provides seven examples of adverse effects on historic properties. The seven examples of adverse effects include:

- (i) Physical destruction of or damage to all or part of the property;
- (ii) Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation, and provision of handicapped access, that is not

consistent with the Secretary's standards for the treatment of historic properties (36 CFR part 68) and applicable guidelines;

(iii) Removal of the property from its historic location;

(iv) Change of the character of the property's use or of physical features within the property's setting that contribute to its historic significance;

(v) Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features;

(vi) Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization; and

(vii) Transfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance.

Constructing a new bridge on the existing alignment north of the current district boundaries, utility relocation, burial, sidewalk reconstruction, streetlight installation, and other associated work will not add any visual, atmospheric, or audible elements that diminish the integrity of the existing district's significant historic features, nor would it detract from its overall historic character. The construction of a new bridge, sidewalks, utility burial, and street lighting will support and enhance both pedestrian access and recreational opportunities, functioning to maintain the human scale, walkability, feeling, design, and association of Northfield Village that has existed historically. As such, the integrity of the District's significant features would remain intact.

Specifically, the proposed streetlights will match existing posts and luminaries currently found throughout Northfield Village (*Figure 11*). Where appropriate, light shields will be installed to prevent high intensity light from affecting buildings, including 38 Main Street. The concrete railing design proposed for the new bridge (*Figure 12*) has been successfully used throughout Vermont in similar downtown settings within historic districts and was Northfield's strongly preferred alternative. Additionally, this concrete railing references the original concrete railings of Bridge No. 60 and will be compatible with the adjacent Depot Square Historic District in materials, design, and other characteristics.

The tree removal, grading, drainage, and retaining wall work proposed at the parcel historically associated with 38 Main St. will not damage or alter any significant character-defining features of the property. To prevent a falling hazard, a 4-foot-tall chain link fence will be installed behind the retaining wall for safety. The existing wooden picket fence will be removed and reset after construction is complete. The picket fence is expected to be aligned with the proposed concrete approach railing, just beyond the sidewalk, similar to how it is situated today. The new, proposed utility pole at this parcel will not introduce new visual, atmospheric, or audible elements. Remaining offset from the main facade of the building, it will appear nearly identical to the extant pole on the parcel currently. VTrans' Landscape Architect has developed a plan to replant portions of this parcel affected and in coordination with the property owner, VTrans will work to

replant an appropriate shade tree to replace the tree being removed – this deciduous shade tree appears older than 50 years of age but it not original to the house or a significant historic character defining feature of the property or historic district.

#### Stipulations

1. VTrans Landscape Architect and Historic Preservation Staff will develop a landscape planting plan for affected areas of the parcel historically associated with 38 N. Main Street, which should include property owner coordination prior to construction.
2. Where appropriate, light shields should be installed on new streetlights to help prevent light pollution at buildings within the project area.

The above information substantiates VTrans' findings and determination of No Adverse Effect for the above project:

*Brennan Gauthier*

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**Brennan Gauthier, Senior Archaeologist**

*Kyle Obenauer*

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**Kyle Obenauer, Senior Architectural Historian**

#### **Attachments:**

- |                                     |                                    |
|-------------------------------------|------------------------------------|
| <input type="checkbox"/>            | Survey Form(s)                     |
| <input checked="" type="checkbox"/> | Photos                             |
| <input checked="" type="checkbox"/> | Map                                |
| <input type="checkbox"/>            | Archaeological Resource Assessment |
| <input type="checkbox"/>            | Other: Plan Sheets                 |

## Images and Illustrations

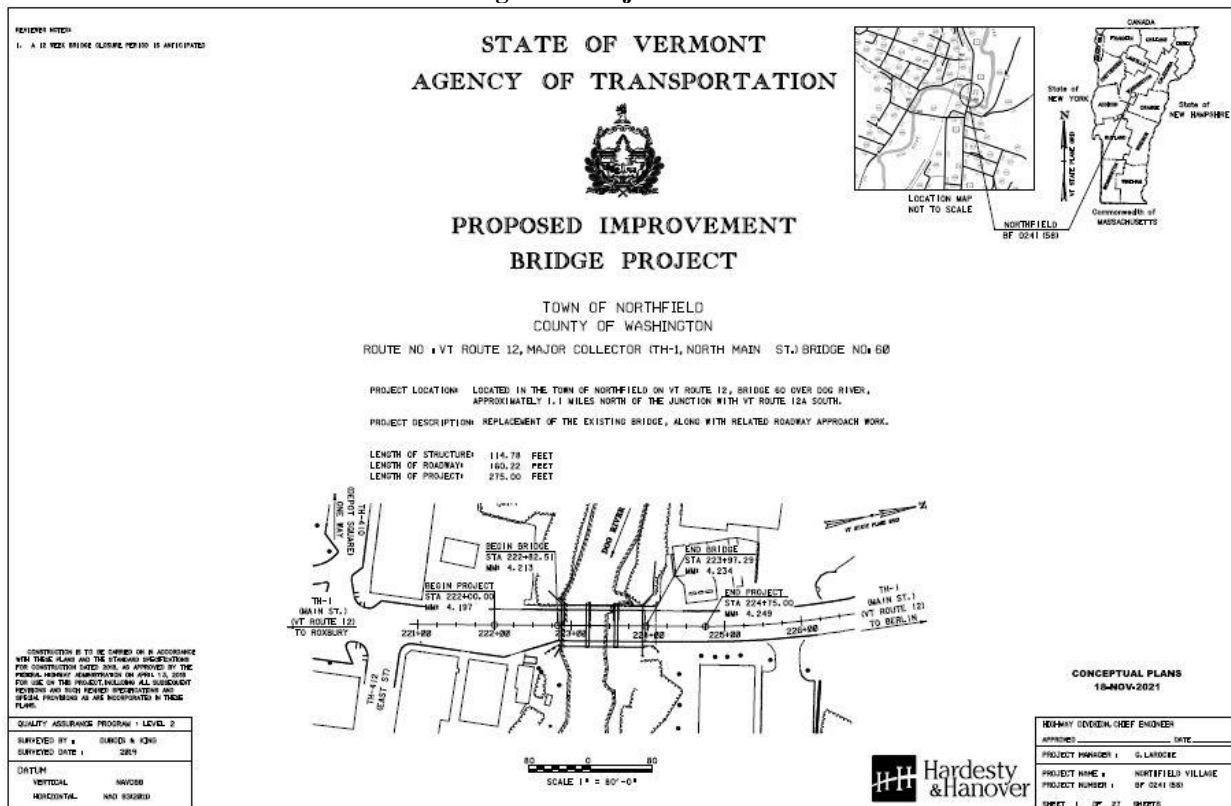
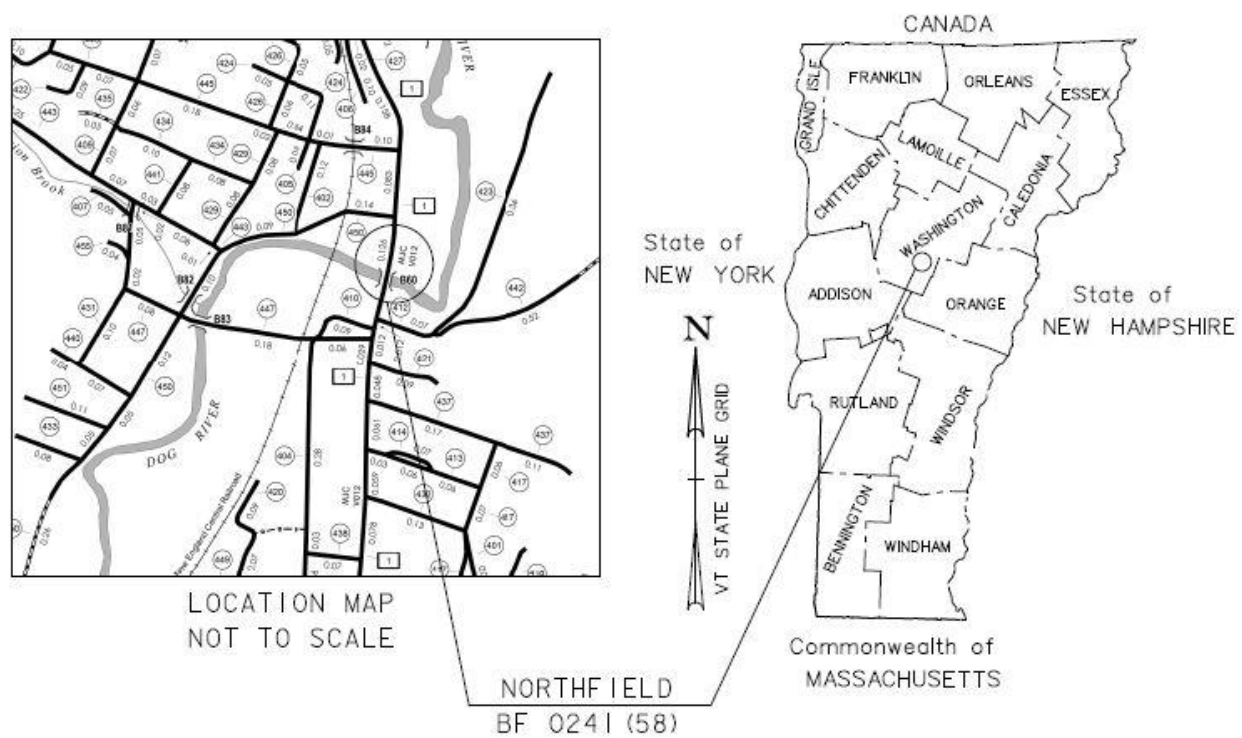
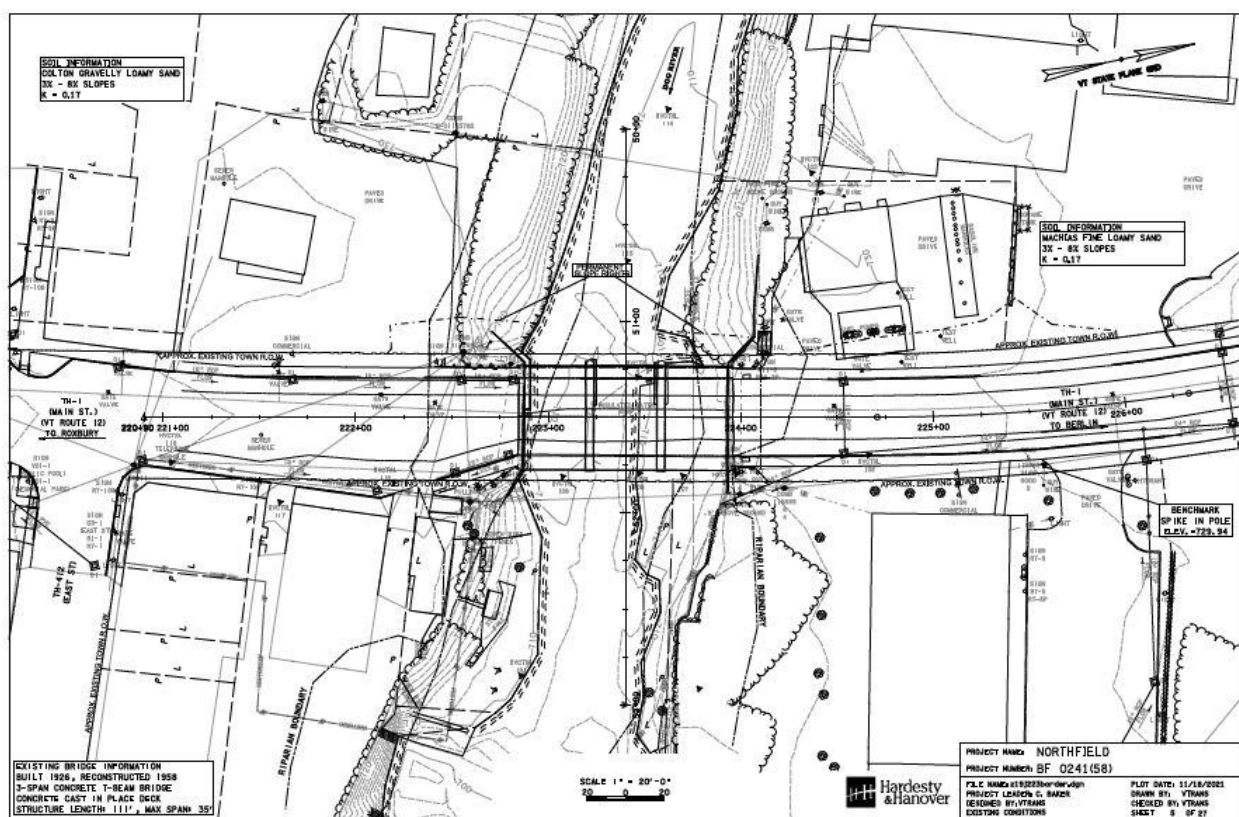
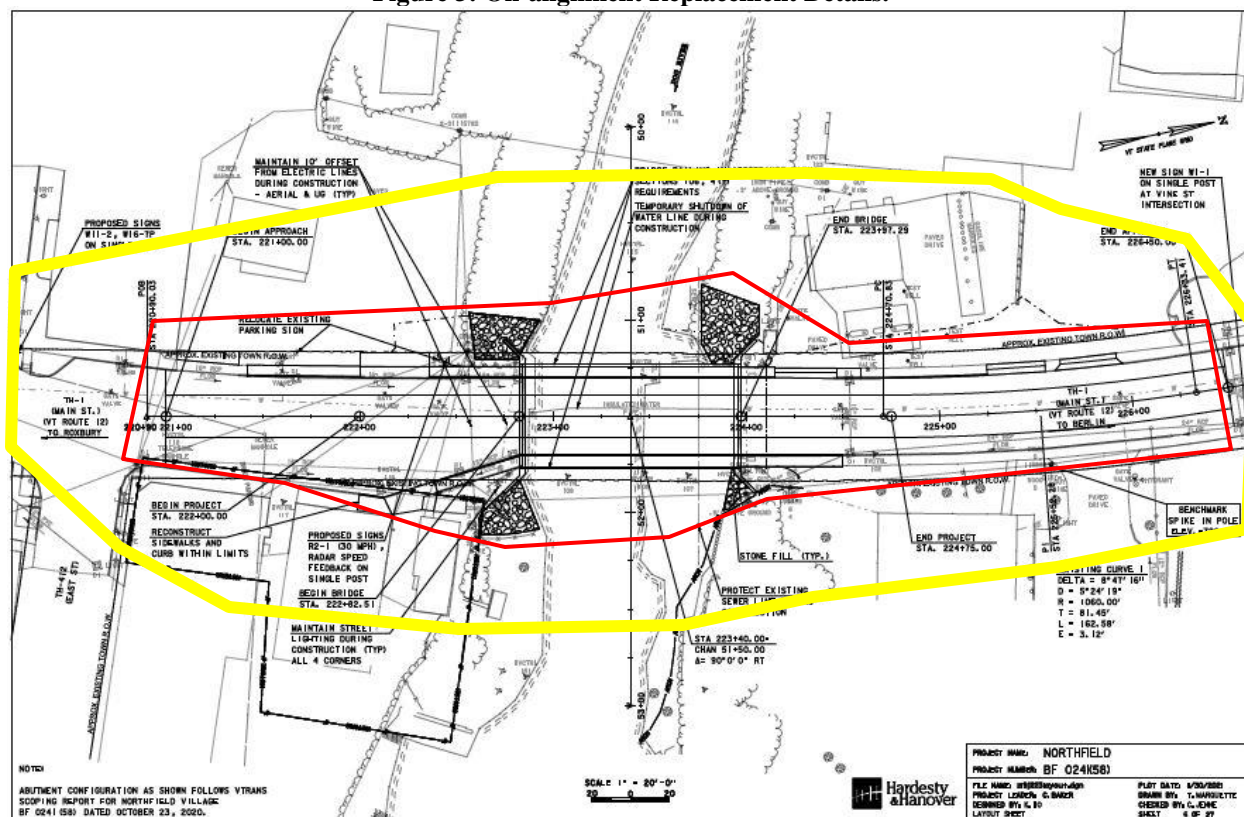


Figure 2: Project Cover Sheet.





### Figure 3: On-alignment Replacement Details.

**Figure 4: Indirect and Direct APE.**



**Figure 5: Bridge No. 60 View West.**



**Figure 6: Bridge No. 60 View North.**

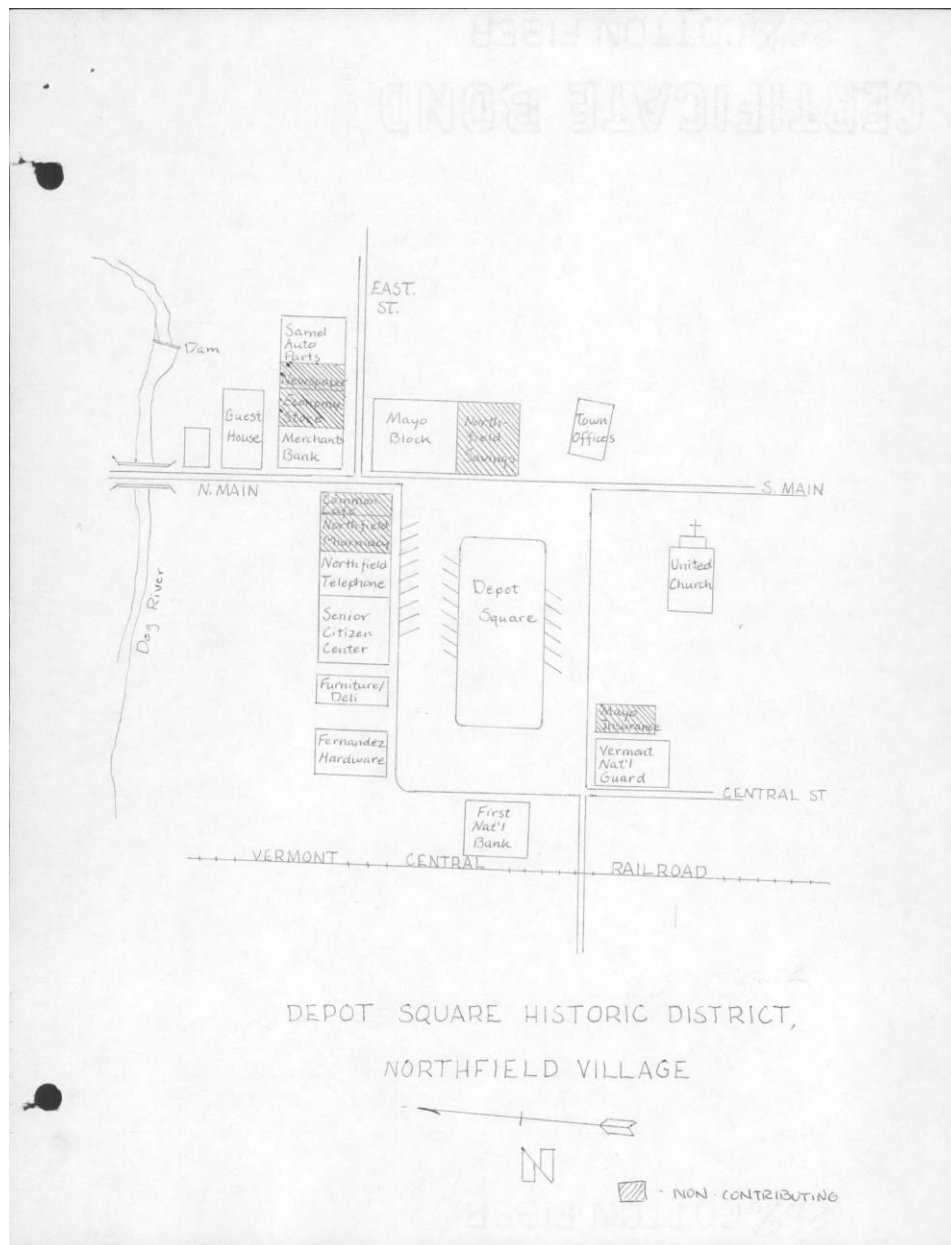




**Figure 7: Bridge No. 60 View SSW.**



**Figure 8: Aerial View of Bridge No. 60.**



**Figure 9. Vermont State Register-listed Depot Square HD**



**Figure 10. Southeastern corner of Bridge No. 60, including 9 East Street, 28 N. Main Street, and 38 N. Main Street.**





**Figure 11. 38 N. Main Street, 28 N. Main Street, and 9 East Street in Northfield Village, left to right. Note extant utility pole at left side of 38 N. Main Street.**



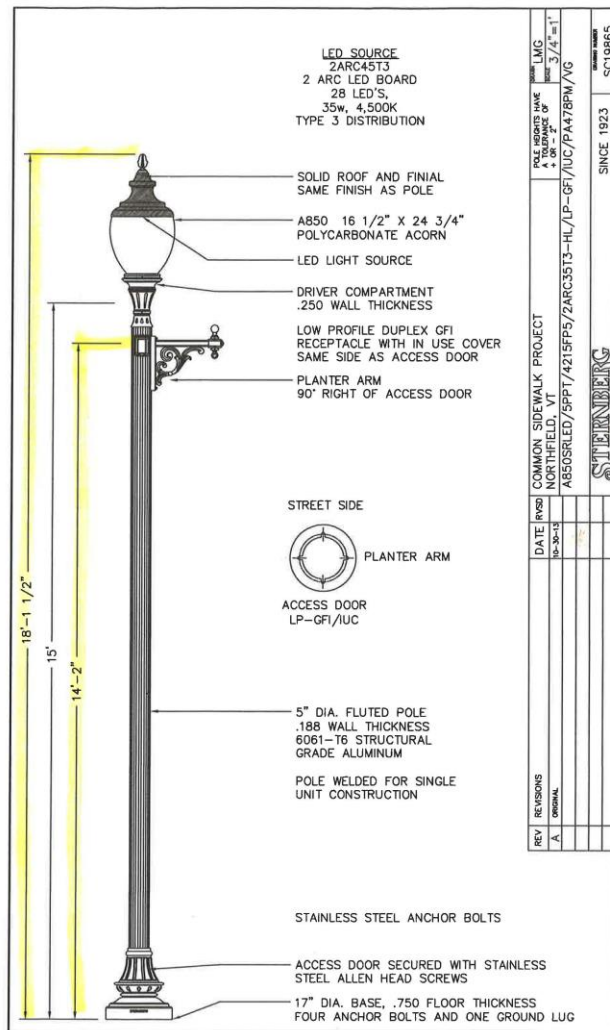


Figure 11. Proposed Streetlight design, matching existing lights in Depot Square.

## Concrete Railing w/ Windows

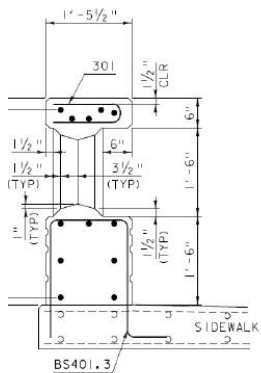


Figure 12. Proposed railing design for new bridge.

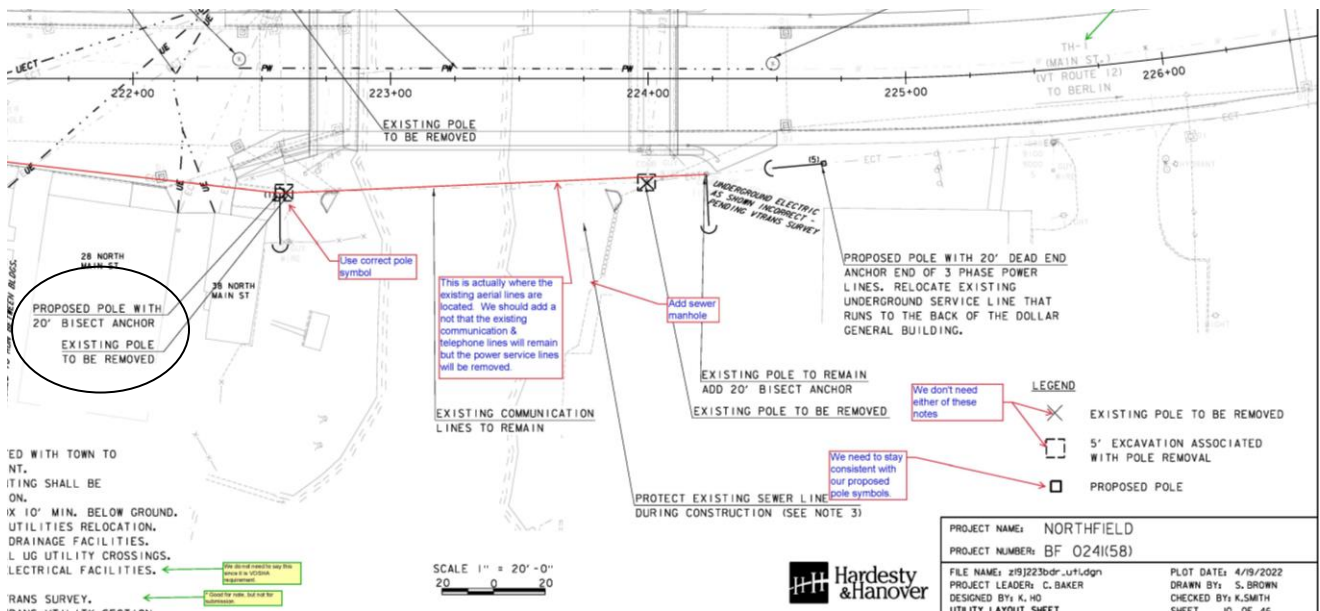


Figure 13. Draft utility relocation plans showing new utility pole to replace existing pole in nearly same location at 38 N. Main Street, Northfield.